

Archaeological Impact Assessment Of Proposed New Car Park Between Stable Lane & Chapel Street (New Lane) Carrick-on-Suir

In Support of a Part 8 Planning Application

Developer: Tipperary County Council

New Street,
Carrick-on-Suir,
Co. Tipperary.

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Date: 29th November 2023



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2040



Comhairle Contae Thiobraid Árann
Tipperary County Council

SUMMARY

Tipperary County Council are submitting a Part 8 Planning Application for the development of a new car park between Stable Lane and Chapel Street (New Lane), Carrick-on-Suir. From the documentary and archival sources it has been concluded that the proposed development is within the archaeological Zone of Notification (ZON) for the historic town of Carrick-on-Suir, reference TS085-004----

Currently, there are 33 known monuments and sites that form the known archaeological resource for Carrick-on-Suir, both upstanding and potentially subsurface. Modern town-plan analysis of the streetscape and plot arrangements in the historic core puts forward a hypothesis that Chapel Street (New Lane) potentially fossilises an earlier town wall layout.

The Ordnance Survey mapping evidence indicates that much of the red line footprint of the proposed car park is located in what were in the 19th, and probably earlier 18th centuries, back gardens and outbuildings such as stables associated with the surrounding buildings. The mapping also indicates the former presence of buildings that have historically been removed, but foundations may remain.

Site inspection of the proposed car park location did not uncover any upstanding fabric or surface indicators of potential subsurface archaeological material, including the buildings shown on the historic mapping. However, without more intrusive assessment through approaches such as archaeological test trenching, the true archaeological potential of the site cannot be fully determined.

Overall, the assessment has determined that the proposed development location has to be regarded as being an area of archaeological potential that requires appropriate archaeological mitigation measures, to progress the development in an archaeologically suitable manner. If left unmitigated, ground works during the car park development have potential to directly and severely impact on the subsurface archaeological resource of Carrick-on-Suir.

It is recommended that the proposed new car park between Stable Lane and Chapel Street (New Lane) Carrick-on-Suir proceed as proposed, with the following mitigation measures.

It is recommended that a programme of archaeological test trenching of the site be conducted, as a pre-development scoping exercise in advance of any construction works. The trenching will be subject to the issuing of the appropriate licence by the National Monuments Service of the Department of Housing, Local Government and Heritage.

It is recommended that prior to any demolition works, a full photographic and drawn record of the standing walls for demolition should be prepared, in mitigation towards their removal.

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1. Introduction

Daniel Noonan Archaeological Consultancy (DNAC) has prepared this Archaeological Desktop Assessment of a proposed new car park to be located between Stable Lane and Chapel Street (New Lane), Carrick-on-Suir, to accompany a Part 8 Planning Application by Tipperary County Council (see **Figure 1-2**).

The proposed car park development is located in an urban block between Chapel Street (New Lane) and Stable Lane, to the north of Main Street in Carrick-on-Suir. It is within the archaeological Zone of Archaeological Notification (ZON) for the historic town of Carrick-on-Suir, reference TS085-004----, as entered in the Record of Monuments and Places (RMP) for County Tipperary (S.R.), and is centred on 640139, 621725 ITM (see **Figures 4-5**). Given the proposed development's location within the ZON, an assessment of the new car park proposal is appropriate, within the context of Policy 13.4 - Archaeology of the *Tipperary County Development Plan 2022-2028*, and the requirements for Notification of the works under Section 12 of the National Monuments Act 1930-2014.

This non-intrusive assessment, based on background research and site inspection, was prepared to evaluate the potential for impacts on the non-renewable archaeological resource, so as to inform the planning process for the proposed development.

The proposed car park development was developed by a multidisciplinary design team, led by Malachy Walsh and Partners (MWP) and Nicholas de Jong Associates Urban Design (NDJ); in association with Tipperary County Council.

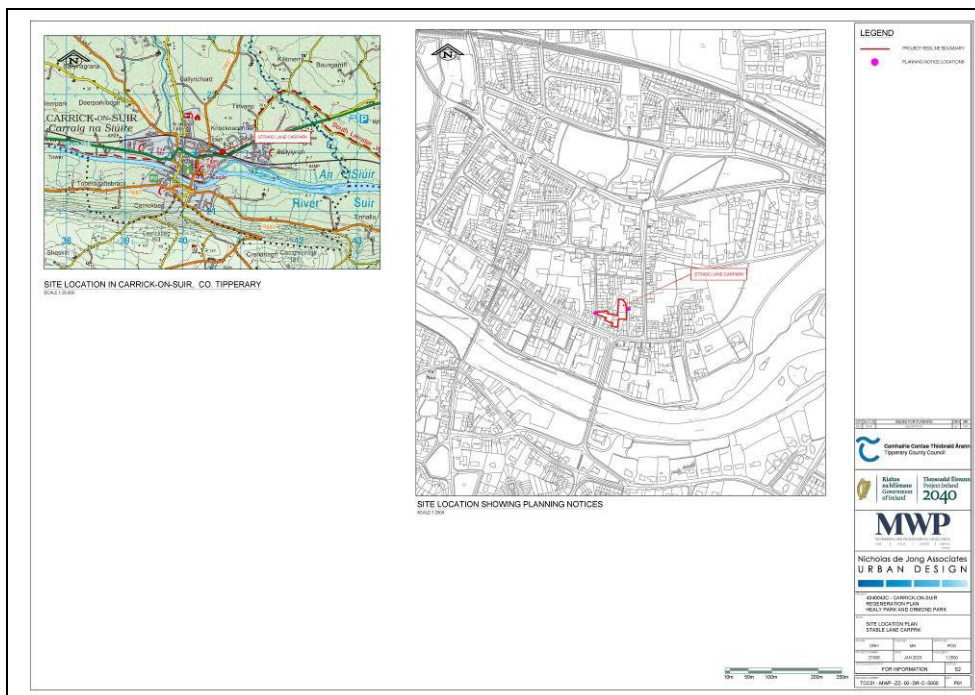


Figure 1: Location of the proposed new car park at Stable Lane/Chapel, north off Main Street, Carrick-on-Suir.

A larger view can be found in **Drawing 01**.

Courtesy of MWP.

2. Assessment Methodology

This non-intrusive assessment of the proposed new car park location between Stable Land and Chapel Street (New Lane), Carrick-on-Suir was conducted through background research into the study area, known archaeological monuments in the locality, historical resources, and mapping. The research is supported by a detailed site inspection and appraisal of the works. The combined aim of this method is to develop an understanding of the archaeological nature of the site, and the potential for impact on the archaeological resource, and how to de-risk this through appropriate mitigation.

The sources consulted include the listings of National Monuments, Preservation Orders, Register of Historic Monuments, the Record of Monuments and Places (RMP), and Sites and Monuments Record (SMR) for County Tipperary (S.R.). Historical and Ordnance Survey mapping, and aerial imagery was sourced. The online databases of the National Monuments Service (NMS), the unpublished *Urban Archaeological Survey of County Tipperary South Riding* (Farrelly and Fitzpatrick 1993), and other documentary sources such as local histories and antiquarian journals were consulted.

The appraisal incorporates the results of the review of the background research, and site inspection, to form the basis of the assessment, and guidance for the recommended mitigation measures.



Figure 2: Aerial view of the proposed new car park, outlined in red.

Source: Google Earth, April 2021.

3. Proposed New Car Park

the proposed new car park off Main Street at Carrick-on-Suir is as follows, as per the Part 8 public consultation notification. The general plan of the works can be found in **Figure 3** below. This section should be read in conjunction with the suite of drawings prepared by MWP and NDJ for the Part 8 application.

Development of new carpark in the townland of Townparks, between Chapel St.(identified as L6711 New Lane on GIS system) and Stable Lane, North of Main St. Carrick-on-Suir, Co. Tipperary to provide convenient off-street parking in close proximity to Main St. This parking will complement the reduction in parking to Main St. resulting from the Carrick-on-Suir Regeneration Plan. The aim of the Carrick-on-Suir Development plan is improvements in active travel, sense of place, and pedestrian and vulnerable road users use. The development of Stable Lane car park aligns with the Carrick-on-Suir Redevelopment Plan by providing separate off street parking area and ensuring that parking is provided within a 5-minute walk of main street and the towns attractions.

The nature and extent of the proposed development is as follows:

- The proposals involve works in the town's Architectural Conservation Area (ACA) and in the vicinity of a number of protected structures.
- Provision of primary vehicular entrance to the West from Chapel St.(identified as L6711 New Lane on GIS system)
- Vehicle exit point to Stable Lane to the east
- Demolition of existing walls, two sheds and concrete yard within the proposed site.
- Demolition of existing boundary wall with Chapel St. and North boundary wall adjacent to Chapel St. to allow reconfiguration of the existing vehicle and pedestrian access points onto Chapel St..
- Demolition of boundary wall with Stable Lane.
- Drainage works to carpark.
- Street lighting to carpark.
- Construction of new Boundary wall to the east of the site along Stable Lane – including vehicle exit point and pedestrian access point.
- Construction of new boundary to the North of the site adjacent to the Chapel St entrance.
- New asphalt surface finishes to carpark, laid to falls and surface water run-off will be directed to gulleys.
- Car park with 33 parking spaces, including two number accessible parking bays and two number electrical vehicles charging bays.
- Bicycle Parking Shelter
- Signage and Line marking
- Landscaping
- All associated site works.

These proposed new car park works will involve ground disturbance within the ZON, and therefore require archaeological mitigation measures to alleviate potential impact on the archaeological resource.

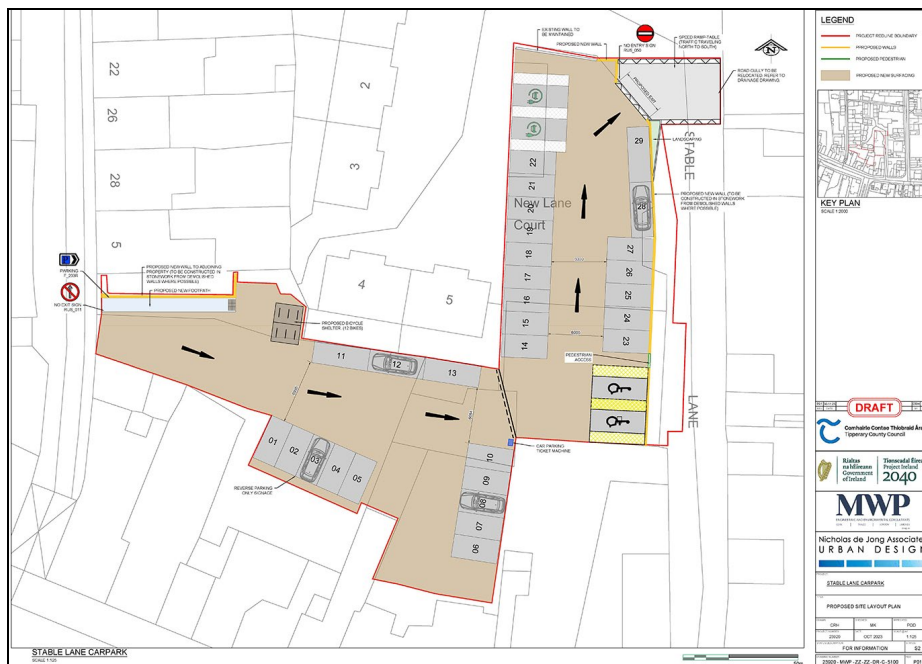


Figure 3: Proposed new car park site layout. Courtesy of MWP/NDJ.

4. Context, Setting & Archaeological Environment

Archaeological Protections & Designations

The proposed works are within the Zone of Archaeological Notification (ZON) for the historic town of Carrick-on-Suir, reference TS085-004----, as entered in the *Record of Monuments and Places* (RMP) for County Tipperary (S.R.). Within the historic town there are 33 known sites or monuments of archaeological significance. Included in these are the complex of medieval and post-medieval castle building at Ormond Castle – a National Monument, reference number 447, and the subsurface line of Carrick-on-Suir's town wall, reference TS085-004032-what surrounds the historic core of the town.

The creation of the Record of Monuments and Places forms part of the current Section 12 of the *National Monuments Act, 1903-2014*; and inclusion in it is the primary mechanism for protection of archaeological sites and monument in the State. Note that the current Act is to be replaced by recently adopted into law *Historic and Archaeological Heritage and Miscellaneous Provisions Act 2023*, but this new legislation is awaiting a Commencement Order to bring it into force. Therefore the older Act has not yet been repealed and remains in force. Under the current, Act any works to, or close by (i.e. within the ZON), a site or monument entered in the RMP requires Notification under Section 12(3) of the current act to the National Monuments Service (NMS) at least two months in advance. In terms of the proposed new car park between Stable Lane and Chapel Lane, the Part 8 Planning Application process will serve as the appropriate notice to the NMS in place of a Notification.

Regarding ground disturbances, the minimum requirement of the NMS is for archaeological monitoring attendance to such works. Further assessment through test trenching is preferred, followed by avoidance of archaeological features to ensure preservation in situ, or preservation by record though excavation, are other potential requirements.

Guidance on current State policy for archaeology can be found in the *Framework and Principles for the Protection of the Archaeological Heritage* (1999) document. The preservation in-situ of archaeological sites and monuments, including archaeological deposits, features and structures, is the preferred option; details of which can be found in Section 3.3-3.4 of the policy document. The use and utility of archaeological assessment as a mechanism for the understanding and protection of the archaeological resource can be found in Section 3.6 of the policy document.

The current policy on the interaction and treatment of town defences and town walls can be found in the *National Policy on Town Defences* (2008) document.

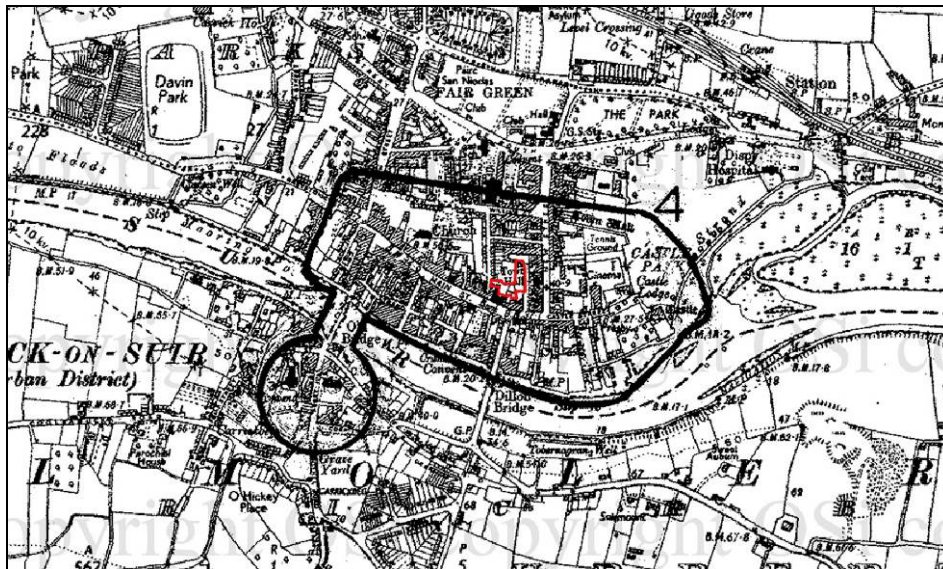


Figure 4: Extract from Tipperary (S.R.) Record of Monuments & Places, 1998, Sheet 085, 1987, showing the Carrick-on-Suir archaeological Zone of Notification (ZON). The location of the proposed new car park is outlined in red. Not to Scale.

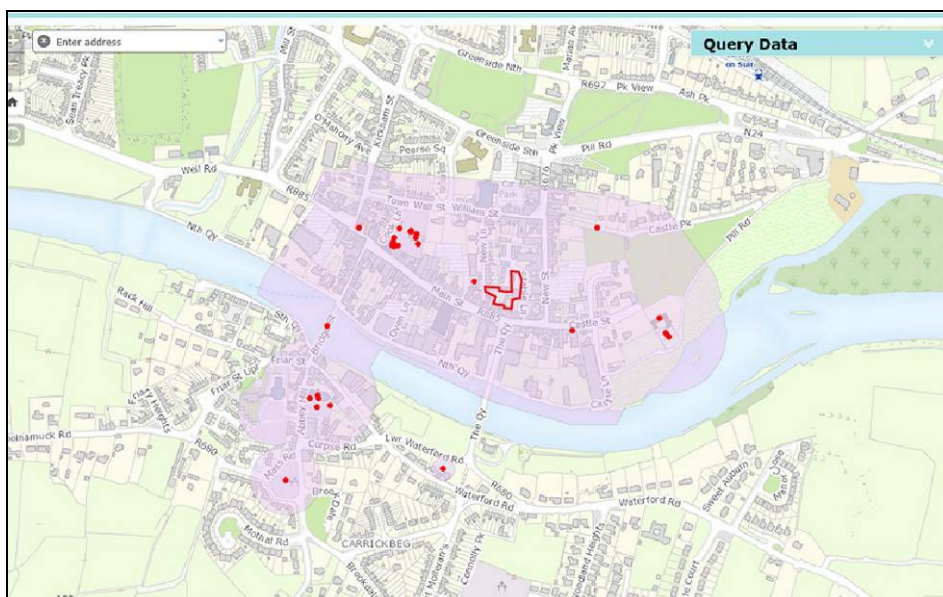


Figure 5: Screenshot of the National Monuments Service's online *Historic Environment Viewer*, showing the Carrick-on-Suir archaeological Zone of Notification (ZON). The location of the proposed new car park is outlined in red. Not to Scale.

Local Authority Development Policies

Tipperary County Council's development policies' regarding archaeological heritage are contained in the *Tipperary County Development Plan 2022-2028*. Planning Policy 13-4 of the Plan regarding archaeology is:

Planning Policy 13-4

Safeguard sites, features and objects of archaeological interest, including Recorded Monuments, National Monuments and Monuments on the Register of Historic Monuments, and archaeological remains found within Zones of Archaeological Potential located in historic towns and other urban and rural areas. In safeguarding such features of archaeological interest, the Council will seek to secure their preservation (i.e. in situ or in exceptional circumstances preservation by record) and will have regard to the advice and recommendation of the Department of Arts, Heritage and the Gaeltacht.

Where developments, due to their location, size or nature, may have implications for archaeological heritage, the Council may require an archaeological assessment to be carried out. This may include for a requirement for a detailed Visual Impact Assessment of the proposal and how it will impact on the character or setting of adjoining archaeological features. Such developments include those that are located at, or close to an archaeological monument or site, those that are extensive in terms of area (1/2 ha or more) or length (1 kilometre or more), those that may impact on the underwater environment and developments requiring EIA.

Carrick-on-Suir Historic Town – TS085-004----

Carrick-on-Suir is a mid-sized market town of medieval origins, located in the Barony of Iffa and Offa East, in Tipperary South Riding. The town historically developed around a strategic crossing point over the Suir River (see **Figures 4-5**). The town is sited just above the floodplain on the north bank of the river, with the suburb of Carrigbeg to the south of the bridge. Carrickbeg is centred on the site of the of the Franciscan friary founded in the early 14th Century. The friary was built on the site on the of the initial manor castle of Carrick-on-Suir; positioned here control the crossing of the river. The main part of the town on the north of the river was walled, but the Carrickbeg suburb does not appear to have been. It is one of four surviving walled towns in South Tipperary, with Clonmel 20km to the west on the Suir, Fethard 24km to the northwest, and Cashel 38km northwest.

In terms of its placename toponymy, Carrick-on-Suir is an anglicisation of the Irish *Charraig na Siúire*, which can literally be translated as 'rock of the Suir', suggesting the presence of a rock outcrop or island in the river that was used as a fording point over the Suir (<https://www.logainm.ie/67177.aspx> - accessed 15/05/2021). It is more historically associated with the name of *Carrickmacgriffin*, when it was held by the Anglo-Norman Griffyn family from the late 12th and into the 13th centuries, before it came into the possession of the leBrets.

The following description of the historic town of Carrick-on-Suir, in the townlands of Townparks and Carrickbeg, is from the National Monuments Service's online Historic Monuments Viewer (<https://maps.archaeology.ie/HistoricEnvironment/> - accessed 15/11/2023):

In the 13th century Carrick-on-Suir, known as Carrickmacgriffin, was held by the Anglo-Norman family, le Brets. Between 1236 and 1246 the priory or hospital of St. John the Evangelist was founded by William de Cantelo and his wife, Dionisia. In 1247 Matthew FitzGriffin received a patent permitting him to hold a fair on Carrigmacgriffin (Power 1989, 20). The Manor became the property of Edmund Butler in the 14th century, the first Earl of Carrick. In 1309 the Butlers built a castle (TS085-004001-) on the site of the Poor Clare convent (TS085-004003-)(Killanin and Duignan 1967, 143). In 1336 James, the first Earl of Ormonde founded a Franciscan friary (TS085-004002-) in Carrickbeg. By 1344 James had obtained a charter from Edward III for the town (ITA Survey 1942, 40/26.5). The earliest surviving borough charter for the town dates to 1366 (Bradley 1985, 42). In 1565 Thomas Butler, Earl of Ormonde, added an Elizabethan manor house (TS085-004002-) to the earlier castle (Killanin and Duignan 1967, 143). In 1650 the castle was captured by Cromwellian forces. Little survives of the town defences (TS085-004032-). The earliest record of the existence of a town wall comes from a grant of 1343 which restored the right to collect murage (Bradley 1985, 40-42). There are a number of other references to the town wall from the 14th century onwards, mainly occurring in property documents (Thomas 1992, vol. 2, 43-6). The town appears to have been enclosed on at least three sides, with the River Suir possibly acting as a natural barrier to the S. However, Thomas (ibid.) suggests that there was also a wall running parallel to the river front. The total area enclosed would have been c. 15 hectares (Bradley 1985, 40-42). Though no town gates survive there would probably have been at least four. The West Gate, at the W end of Main Street, survives as a placename and it is possible that there was an E gate at the opposite end of this street (Thomas 1992, vol.2, 43-6). It is also probable that there was a gate on New Street where it intersects with the town wall and another gate at the S end of Bridge Street (Bradley 1985, 40-42). Other medieval remains in the town include the 'Old Bridge' (TS085-004007-) and an urban tower house (TS085-004008-). The medieval church (TS085-004006-) of St. Nicholas of Myra does not survive above ground. Nor is there evidence above ground of the priory or hospital of St. John the Evangelist mentioned above. According to Bradley (1985, 42) 'the foundation charter states that it was beside the Suir and the 'garden of the Brethern of St Thomas the Martyr', referred to in a deed of c. 1300, indicates that it lay on the south side of the town, most likely adjoining the river'. It later became subject to the Hospital of St. Thomas of Acon, London (Lee 1966, 12-26) and in 1557 the 'priory' was granted to Thomas Butler, Earl of Ormonde (Gwynn and Hadcock 1970, 347). References dating to the 15th century to a street leading towards 'le Spedell' suggest that there may have been another hospital on the W side of town (Bradley 1985, 42). There was a frankhouse of the Knights Hospitallers under Hospital of Any referred to in 1541 (Gwynn and Hadcock 1970, 340), however, the location of this building is unknown. A list of properties granted to the Duke of Ormonde in 1668 provides good information on the types of houses in Carrick-on-Suir in the late 17th century, as well as the names of the occupants (Manning 2008, 7-20).

Medieval Town Layout

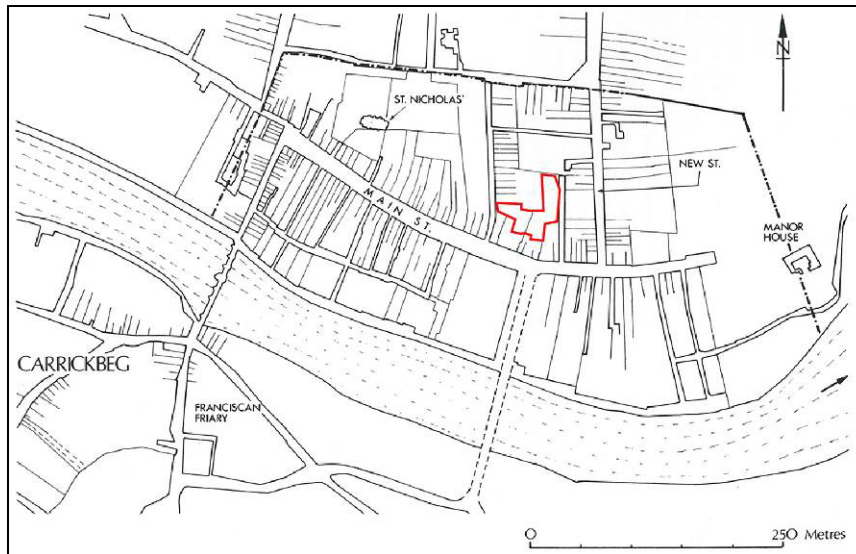


Figure 6: The medieval town of Carrick-on-Suir, by John Bradley (Bradley 1985(b), 40).

The location of the proposed new car park is outlined in red.

The historic layout of the medieval town of Carrick-on-Suir remains fossilised in its modern streetscape (see **Figure 6**). With a linear layout, the town was centred on the Main Street, running east to west as the central spine of the roughly rectangular walled area. As it is today, Main Street was the main medieval throughfare or 'King's Highway'; and the widening of the street towards the west marks the location of the medieval market place. The market place may have had a market cross, and other structures such as toll booths or exchange buildings that were sited on it. The extents of the burgage plots of the medieval properties are preserved in the footprint of many of the properties of the town. Those on the north side of the Main Street ran as far as the line of the town wall, and those on the south ran to the edge of the Suir, which is fossilised in the line of Strand Lane.

On the slightly higher ground to the northwest the medieval parish church of Saint Nicholas's was located; protected within the northwest corner of the town wall. As the modern Town Wall Street placename indicates, that street follows the east to west line of the town wall. Having formed initially as a path or track around the outside of the wall in post-medieval times, when the town wall lessened in importance as a defensive feature and development began outside the walls, the route became a street. Beyond Town Wall Street to the east, the external route outside the wall became fossilised as today's William Street; as far as the junction with New Street, which as its name suggests appears to be an addition to the medieval layout, with a landward gate called New Gate. In a similar fashion, Kickham Street formed outside the western run of the wall, north of the west gate.

Internally, there were numerous laneways, several of which remain open, some narrow as they would have been originally, some widened into more prominent streets of today, and include routes such as Hotel Lane, Jones's Lane, Oven Lane, O'Callaghan's Lane, and Brewery Lane. These lanes ran south from the Main Street to access Strand Lane; as they did in medieval times when the edge of the Suir was further inland, following the east to west run

of Strand Lane. Strand Lane marks the southern extent of the enclosed medieval town. It may have been walled (Thomas 1992, Vol. 2, 45), and the riverbank may have contained outworks or jetties into the river, and landing points, to support the commercial trade of the town. It is unknown, but there may have been formal riverside gates along the line of Strand Lane that controlled this trade. Laneways on the north side of Main Street gave access to the rear of the burgage plots into which the medieval town was divided, and provided access to locations such as the parish church of Saint Nicholas's, and to the northern run of the town wall.

From the early 14th Century onwards a stone bridge (the 'Old Bridge') stood at the foot of Bridge Street, providing access to Carrigbeg to the south, and landward passage from the south.

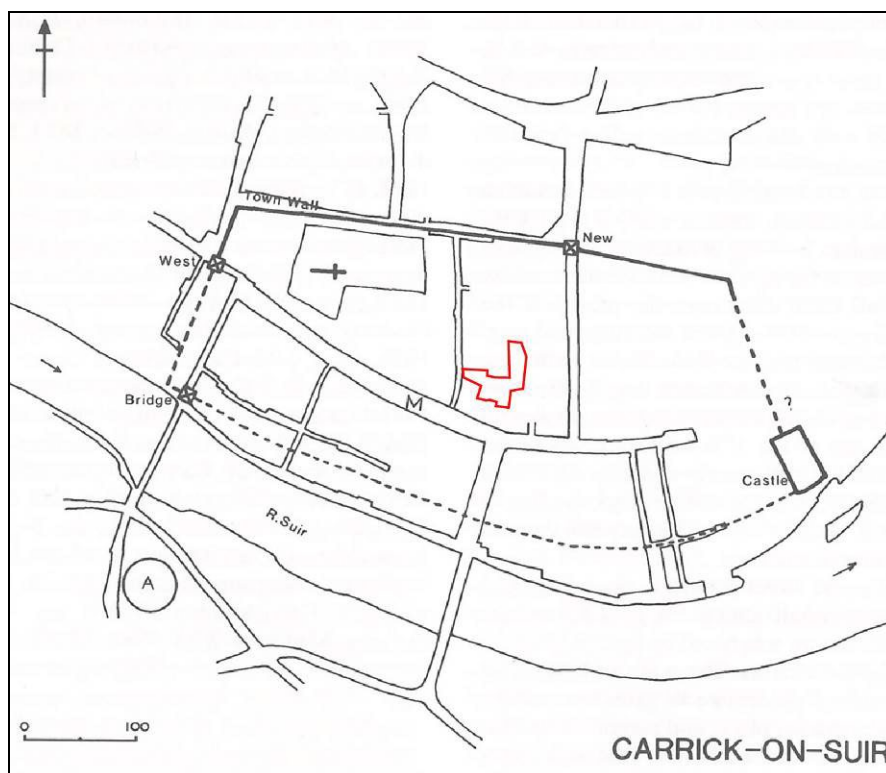


Figure 7: The walled town of Carrick-on-Suir, by Avril Thomas (Thomas 1992, Vol. 2, 44). The location of the proposed new car park is outlined in red.

Carrick-on-Suir Town Defences – TS085-004032-

Carrick-on-Suir had a stone town wall from at least the early 14th Century (Bradley 1985(b), 40-42; Thomas 1992, Vol. 2, 45), which was roughly rectangular in shape, on the north side of the Suir, and encompassed an area of up to 15 hectares (**Figure 7**). It was walled to three sides, west, north, and east; and possibly along the south side by the Suir, although this is debated (ibid.). The potential route of the town wall within the present-day townscape is set-out in the *Carrick-on-Suir Town Walls Conservation, Management and Interpretation Plan* (2013). The medieval line of the Suir was further north than today's quayside, and this line, and that of any probable wall, is fossilised in the line of Strand Lane, which is an average of 40m north of the current quayside wall. There were four gates to the medieval town: at the western end of the Main Street at West Gate, as memorialised by the placename; a

probable gate at the eastern end of Main Street, close to the castle; at the junction of New Street and William Street, i.e. the New Gate; and to the north of the medieval bridge, a Bridge Gate in the vicinity of the meeting of Strand Lane and Bridge Street. The wall had at least 3-4 towers, and it is not known if it had an external fosse or ramparts.

Known Archaeological Monuments

The following **Table A** contains a listing of the 33 known monuments and sites that form the archaeological core of Carrick-on-Suir; garnered from the online *Historic Environment Viewer* of the National Monument Service's database, accessed 15/10/2023, - <https://maps.archaeology.ie/HistoricEnvironment/>.

Table A: Recorded Monuments in Carrick-on-Suir's Historic Town Core.

SMR/RMP No.	Classification	Comment
TS085-004----	Historic town	The medieval town of Carrick-on-Suir.
TS085-004001-	Castle - Anglo-Norman masonry castle	Early 14 th Century castle, first in the three phases of castle building in the Ormond Castle complex.
TS085-004002-	House - 16 th century	Tudor Mansion, built in 1565 by Thomas Butler, 10 th Earl of Ormonde, as the final phase of castle building in the Ormond Castle complex.
TS085-004003-	Religious house - Franciscan nuns (Poor Clares)	The Butler Castle now stands on the site of the convent.
TS085-004004-	Religious house - Franciscan friars	Franciscan friary founded in the early 14 th Century, on the site on the of the initial manor castle of Carrick-on-Suir, on the south side of the river in the suburb of Carrickbeg.
TS085-004005-	Castle - unclassified	Site of the initial 13 th Century manor castle, possibly a motte-and-bailey type, on the south side of the river crossing, in Carrickbeg. No upstanding remains.
TS085-004006-	Church	Site of former medieval parish church, dedicated to Saint Nicholas of Myra; located to the north of the west end of the Main Street. Now occupied by the early 19 th Century former Church of Ireland church, which operates as a heritage centre.
TS085-004007-	Bridge	Eight segmental arched, stone built bridge over the Suir, which may contain fabric from as early as the 14 th Century.
TS085-004008-	Castle - tower house	A four-storey urban tower house, known as the Tholsel, located to the immediate north of the site of the West Gate. Traditionally, a tholsel housed local administrative functions and collected tolls and taxes.
TS085-004009-	House - 17 th century	A distinctive stout projecting chimney and steep pitched roof suggest that this building at the corner of Castle Street and Brewery Lane is of 17 th Century date.
TS085-004011-	Church	<i>Relig na muc</i> , or the Pig's Cemetery, associated with the old church of Killmoleran; in Carrickbeg, on the junction of the Mothel and Mass roads. Same entry for TS085-004031-.
TS085-004013-	Bawn	Bawn wall associated with the 15 th Century tower houses added to the Ormond Castle complex.
TS085-004014-	Graveslab	Inscribed 16 th / 17 th Century graveslab in the base of the south wall of Satin Molleran's Church, on the site of the Franciscan friary.
TS085-004015-	Architectural feature	Possible window head fragment, from the demolished medieval parish church, now incorporated into a wall in the church yard.
TS085-004017- to TS085-004021-	Graveslab	Multiple decorated fragments of limestone medieval graveslabs, located in the church yard of the former Church of Ireland church.
TS085-004022-	Tomb - chest tomb	Side panel of a medieval altar tomb, incorporated into the western boundary wall of the former Church of Ireland church/site of the Saint Nicholas's medieval parish church.
TS085-004023- to TS085-004030-	Graveslab	Multiple decorated fragments of limestone medieval graveslabs, located in the church yard of the former Church of Ireland church.
TS085-004031-	Graveyard	<i>Relig na muc</i> , or the Pig's Cemetery, associated with the old church of Killmoleran; in Carrickbeg, on the junction of the Mothel and Mass roads. Same entry for TS085-004011-.
TS085-004032-	Town defences	Roughly rectangular shaped town wall, enclosing an area of 12-15 hectares; walled on at least three landward sides, and possible wall along the riverside.
TS085-004038-	Graveyard	Graveyard associated with Saint Molleran's church, on site of the cloister associated with the Franciscan friary.

TS085-004039-	Tomb - effigial	Female effigy against the inner face of the graveyard wall of Saint Molleran's.
TS085-004040-	Wall monument	Late 17 th Century wall monument from the medieval Saint Nicholas's, now in north wall of the Church of Ireland church/heritage centre.
TS085-004041-	Graveyard	Graveyard of the medieval parish church of Saint Nicholas's.
TS085-024---	Bullaun stone	Moved here from Reatagh townland in Waterford.

Archaeological Excavations

There have been multiple development-driven archaeological investigations and excavations in the ZON for Carrick-on-Suir in recent years.

In preparing this appraisal, the online excavations database resource (<https://excavations.ie/mapsnew/> - accessed 15/10/2023) was consulted, and the following, non-exhaustive selection of 10 investigations, presented in tabular form (**Table B**), was garnered from sites relevant to the assessment of the Carrick-on-Suir regeneration scheme; particularly works on the streetscapes and in vicinity to known monuments.

Table B: Results of project relevant archaeological investigations in Carrick-on-Suir, 2001-2020.

Licence/Reference	Location	Findings & Appraisal Comment
07E0689/2007:1604	21 Chapel Street (New Lane), Carrick-on-Suir	<p>Monitoring of groundworks associated with a residential development at 21 Chapel Street, Carrick on Suir, Co. Tipperary, took place on 30 July 2007. Orientated east-west, the site is a long narrow strip of land which had a house fronting on to Chapel Lane. The eastern half of the site consisted of garden soil and the western half consisted of rubble from the demolition of the initial 1930s house.</p> <p>The excavation of the foundation trenches was carried out with an 8-tonne rubber-tracked mechanical excavator fitted with a grading bucket measuring 1.6m in width. The trenches were excavated to a depth of 0.5m below the current road (Chapel Street) level. The house foundations were excavated and no archaeological features were uncovered. Excavation took place in the western area of the site only. The trenches extended 13m east from the front of the site. In total 50m of foundation trenches were opened at the site. The eastern extent of the site will remain undisturbed as it will remain a garden. No features or deposits of an archaeological nature were identified during the monitoring programme.</p> <p>Comment – these works give a useful profile of the archaeological potential of the street.</p>
Consent C003451 /2007:1603	Multiple locations across Carrick-on-Suir	<p>Groundworks were monitored as part of laying ducting for a broadband scheme in Carrick-on-Suir town. Little of archaeological significance was uncovered. Substantial sections of the works within the zone followed routes that had previously been disturbed to accommodate services, most notably the Carrick-on-Suir main drainage scheme. Almost all groundworks in Carrickbeg extended along the main drainage corridor. Regarding the town centre, ducting along New Street followed almost all the route of a service laid in the 1980s. Along the Main Street ducting extended on the opposite side of the street to the main drainage scheme, but nothing of archaeological provenance was uncovered. The closeness of the natural deposition to the present ground level was noteworthy, in some instances occurring beneath the road make-up. This suggests any previous road surface was removed at the time of the present road being laid, or that the earlier surface comprised the natural clays and was never cobbled.</p> <p>At two locations the line of the town wall/fortified gates, classified as a national monument, was crossed: the West Gate and New/North Gate. Excavations for ducting were kept very shallow (0.4m below modern ground level) to avoid encountering the medieval walls; the trench was metal sheeted in these areas.</p> <p>Comment – these works give a useful profile of the archaeological potential of the streets, confirming past impact and disturbances. However, as the works were kept shallow on the sites of the West Gate and New Gate, the potential for remains of the</p>

		town wall at those locations was not established.
14E0038/2012:149	76 Main Street, Carrick-on-Suir	<p>Following the discovery of 81 gold coins during building restoration works at 76 Main Street, Carrick-on-Suir, Co. Tipperary, an archaeological assessment and metal detection survey was carried out to further assess the find site by Shanarc Archaeology Ltd. Seven test trenches were excavated across the development site, trenches 1-6 were located within the northern extent of the building site while the 7th trench was situated along the internal eastern boundary wall where the coins were found. The hoard comprised 81 gold coins dating to the reigns of Charles II (1660-85), James II (1685-8), William and Mary (1688-94) and William III (1694-1702). The earliest coin dates to 1664, and the latest to 1701. It can therefore be assumed that the coins were buried after the year 1701 or perhaps added to the hiding place between 1664 and 1702. According to the workmen who made the discovery, the coins were laid out in a line under a layer of soil beneath the floor level of the building. The hoard is thought to represent the accumulated wealth of a single family that once lived in the building. Documentary sources (such as the 1654-56 Civil Survey and the 1799 Census of Carrick-on-Suir) do not record who occupied the premises in the 17th and 18th centuries. Griffith's Valuation records that in 1853 Charles Fennessy owned Nos. 76, 77 and 78, Main St. Each property is described as a 'house, office and yard', and No. 78 had a garden. No. 76 is described as 'vacant'; Nos. 77 and 78 were leased by John Lowry and Patrick Callaghan, respectively. No. 75 Main St, held by James Moore, is listed as being a weigh-house.</p> <p>Comment – this interesting find confirms the artefact baring potential for works in Carrick-on-Suir.</p>
Consent C000727 /2017:032	Town Wall Street	<p>Summary text - Monitoring was undertaken of footpath renewal works along a section of Town Wall Street, Carrick-on-Suir. Carrick-on-Suir Municipal District undertook the replacement of a section of a footpath along a length of the south side of Town Wall Street. The purpose of the works was to replace a damaged section of footpath with new paving. As the works were located within the immediate vicinity of the medieval town defences – a National Monument – it was necessary to obtain a Ministerial Consent. The line of the medieval town defences extends along Town Wall Street.</p> <p>The works entailed the removal of the damaged concrete footpath for a total length of 33.4m along the south side of Town Wall Street. Excavations were kept very shallow, no more than removing the existing footpath and very limited sub-surface ground works.</p> <p>No archaeological features, and in particular no trace of the town defences, were uncovered. Given the shallowness of the works this was not unexpected.</p> <p>Comment – this acknowledges the potential for the Town Wall to be encountered in this location, and mitigated potential impacts by avoidance through keeping ground disturbance shallow.</p>
Unlicensed/2018:842	New Street	<p>Summary – Street frontage construction works at the Town Hall and New Street carpark, New Street, Carrick-on-Suir, were undertaken on behalf of Tipperary County Council as part of the refurbishment of the Town Hall and the provision of a civic plaza; these works were archaeologically monitored during October and November 2018.</p> <p>Work was carried out in an area disturbed by urban infrastructure and sub-surface services; monitoring facilitated the identification of post-medieval remains associated with a British Military Barracks that formerly occupied the New Street car park. While little has been learned of the Military Barracks to date, it was present on New Street in the 18th century, and remained at this site into the 20th century. Remains associated with the Barracks generally lie 0.2m below the car park surface.</p> <p>Little upstanding fabric associated with the Barracks remains, with the exception of sections of boundary wall. A remaining section of boundary wall, at the New Street frontage at the south side of the car park, was further partly demolished during the construction works. Monitoring showed that sub-surface remains of the street frontage boundary wall do survive, but in a heavily disturbed state, in consequence of multiple phases of service laying. Floor surfaces associated with former Barrack</p>

		<p>buildings were also recorded in the south-west corner of the New Street car park.</p> <p>An archaeological deposit of potential interest – a black, charcoal-rich deposit overlying a cobble surface – was identified between 0.5m and 0.75m below the street level when excavating a water drainage trench parallel with New Street at the north end of the works area. The deposit and cobbled surface, partly disturbed to the west from the insertion of earlier services, remain in situ below the new water drainage pipe. Any future works involving excavations of >0.5m below the street level in this area of New Street and New Street carpark have the potential to re-expose or impact the surviving charcoal deposit and cobbled surface.</p> <p>Excavations produced a very small number of post-medieval finds, consisting principally of 19th-20th-century pottery sherds and 18th- and 19th-century clay pipe fragments.</p> <p>Comment – despite the disturbed nature of the street, due to existing services, the attendance did note the presence of earlier cobbled surface at a depth of 0.5m below the present ground level that is of archaeological significance.</p>
Unlicensed/2018:843	New Street	<p>Summary - Archaeological monitoring of excavation work associated with the upgrading of the existing water pipe infrastructure on New Street, Carrick-on-Suir, Co. Tipperary, was undertaken from 19 to 26 June 2018.</p> <p>A pair of trenches, of approximately 42m in length, were excavated to a depth and width of approximately 1m each, running north-south parallel to one another along New Street, in the vicinity of the town hall to the west, and the site of the former military barracks to the east. The surface road layers were broken through to a greyish-brown silty clay with frequent small and medium stone inclusions. In places gravel deposits lay underneath the road surface.</p> <p>Nothing of archaeological significance was identified as part of the excavation.</p> <p>Comment – in contrast to the earlier phase of monitoring, nothing of interest was noted.</p>
Unlicensed Monitoring/2019:663	Abbey Hill Road /Bridge Street /Dillon Bridge /Castle Lane	<p>Summary - Footpath reconstruction works undertaken on behalf of Tipperary County Council in the historic town of Carrick-on-Suir were archaeologically monitored from 1-5 November 2018 and from 14-15 January 2019.</p> <p>Works were carried out in numerous locations around Carrick-on Suir, but were principally concentrated around Dillon Bridge, Bridge Street, Castle Lane and Abbey Hill Road, all within the zone of archaeological potential of the historic town (TS085-004).</p> <p>No features, deposits or finds of archaeological interest or significance were exposed or disturbed during the course of footpath reconstruction works at Dillon Bridge, Bridge Street and Abbey Hill Road. This was to be expected given the shallow nature of the works, and the fact they took place in footpaths heavily disturbed by urban infrastructure and sub-surface services.</p> <p>At Dillon Bridge, a sub-surface limestone wall footing was identified during the course of digging around the base of an existing street light. The footing extended below the level of the bottom course of the worked limestone wall that retains the road leading onto the bridge. The unworked limestone, which is mortared, was partially dug through to facilitate the original installation of the street light.</p> <p>A compact clay surface and masonry remains were recorded below the surface at the entry to the 'Strand Walkway'/Pill Road on the east side of Castle Lane. It has not been possible to establish the origin of the clay surface and masonry remains, although the presence of red brick below the clay surface and in the joints of a probable wall suggests a post-medieval date. 19th-century Ordnance Survey maps do not show a structure on the street side of the walls bordering the 'Strand Walkway'/Pill Road, and it is possible the remains predate the Survey. It is assumed the masonry remains extend westward into Castle Lane.</p> <p>Findings consisted of a small amount of post-medieval pottery and glass sherds.</p> <p>Comment – two items of note from this investigation: (a), the town wall or any possible gate features remains were not encountered at Bridge Street, given a combination of shallow construction formation levels and previous disturbance; (b), groundworks for the upgrades to the pathway at Pill Road (Strand Walk), south of</p>

		<p>the Ormond Castle complex, has potential to encounter the undated clay surface and masonry remains uncovered at the 'Strand Walkway'/Pill Road on the east side of Castle Lane.</p>
20E0629/2020:211	New Street	<p>Summary - Tipperary County Council is undertaking service duct and footpath replacement works on New Street, Carrick-on-Suir, Co. Tipperary (Carrickbeg townland). The project entails service trench excavation for reinstatement of services (drainage and ESB), realignment of same, excavation for resurfacing, and new paving finish works including kerbing, throughout New Street. These linear works measure some 140m north to south by 10m and are set firmly within the urban core of the town. The deeper works will entail achieving c. 1m in depth and run the length of the works on either side of the street, which includes four road crossings. Shallower works, i.e. footpath and kerb replacement, will entail achieving depths of up to 0.4m, the majority of which is anticipated to be up to 0.3m.</p> <p>The project has been partially completed with exposed stratigraphy being either modern or 19th-century building debris. To date no archaeology has been encountered and the works are set to resume in 2021.</p> <p>Comment – the results confirm that the potential for archaeological material can vary in the town.</p>
Unlicensed Monitoring/2020:336	New Street	<p>Summary - Unlicensed monitoring was carried out at New Street, Carrick-on-Suir, Co. Tipperary on 21st May 2020.</p> <p>Two small slit trenches sited within the public footpath to the east side of New Street, Carrick-on-Suir, were excavated as part of advance site investigation work associated with a Council proposal to install existing overhead power lines below footpath level on the east side of the street. The slit trenches were orientated east-west across the public footpath, perpendicular to, and outside of, No. 23 New Street (ST1) and outside No. 13 New Street (ST2).</p> <p>The remains of a cobble stone surface, surviving 0.4m below the surface of the footpath, were uncovered in slit trench 1. No other features or deposits of archaeological significance were identified. Gravel deposits above and below the cobble stone surface contained fragmented animal bone, no additional finds of archaeological significance were recovered.</p> <p>Comment- the cobble surface is similar to that uncovered further to the north in 2018, and may be part of same historic street surface; and given the depths it was encountered at, may be medieval in origin.</p>
Consent C001187/Licence 23E0503	Multiple locations across Carrick-on-Suir	<p>A programme of Site Investigation (SI) to support the development of the detailed engineering design for forthcoming public realm works associated with the Carrick-on-Suir Regeneration Plan was conducted under archaeological monitoring conditions; with Consent C001187 in place for works in the vicinity of the town wall and licence 23E0503 for works elsewhere within the ZON.</p> <p>In total twenty (20) trenches were monitored on Strand Lane, Bridge Street, New Street/Chapel Street, Greystone Street and Main Street.</p> <p>During the monitoring no medieval archaeological features were uncovered across the small array of small test trenches. Four features, three minor walls and cellar, were uncovered and are modern in origin and small scale in nature. It would appear that the streetscapes of Carrick-on-Suir, like all urban centres, has seen significant impact from modern services and surface treatments. However, Carrick-on-Suir is a walled medieval town, and the potential for groundworks associated with the Carrick-on-Suir Regeneration Plan to impact on unknown and unpredictable subsurface archaeological features cannot be absolutely ruled out.</p> <p>It is recommended that groundworks associated with the Carrick-on-Suir Regeneration Plan be subject to archaeological monitoring.</p> <p>Comment – these works give a useful profile of the archaeological potential of the streets of Carrick-on-Suir.</p>

The earliest historic mapping for Carrick-on-Suir is that by Nicholas Pynnar, as part of his survey of the fortifications of Ireland in his role as surveyor of forts in 1624 (see **Figure 8** for the reproduction of the Pynnar map for Philip Hore's Volume 3 of the *History of the Town and County of Wexford* 1901). The view shows the rectangular town, walled all-round and to the river, with the bridge over the Suir, and a fortification on the southern bank in Carrickbeg.

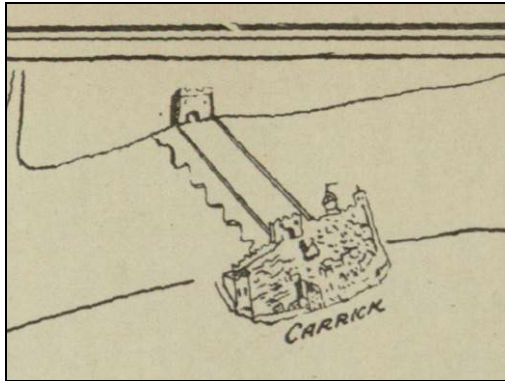


Figure 8: Pynnar's view of Carrick-on-Suir in 1624.



Figure 9: Carrick-on-Suir as depicted on the Tipperary County map of the Down Survey, 1656-58.

The Down Survey of 1656-58, shows the town as a rectangular walled space on the north side of the Suir, with four towers, internal structures including the parish church, and the bridge over the river (**Figure 9**). To the west the land is shown as forested.

The fragmentary depiction of the town in the *Map of the town of Carrickbeg, Co. Waterford, belonging to James Dk of Ormonde by John Taylor – 1699* confirms the burgage layout of the town, running north and south from the Main Street or Kings' Highway (**Figure 10**).



Figure 10: Part of John Taylor's 1699 map, from the Ormond Castle visitor's guide (Fenlon 2008, 42).

By 1790 the line of the town wall on the riverside appears to have been removed, and the town expanded into the river beyond the line of Strand Lane – probably a name acquired from riverine sand or gravel that lapped the south side of the town before it was developed. The aforementioned historic mapping sources are largely illustrative in nature, illustrating information on the general form and development of Carrick-on-Suir. They are not of sufficient detail to show the location and development of Stable Lane and Chapel Street

(New Lane). It is with the work of the Ordnance Survey that the first useful mapping of scale for the vicinity of the proposed new car park off the Main Street becomes available.

By the time of the First Edition of the Ordnance Survey of the late 1830s' the town of Carrick-on-Suir had developed well beyond the confines of the medieval town wall (**Figure 11**).

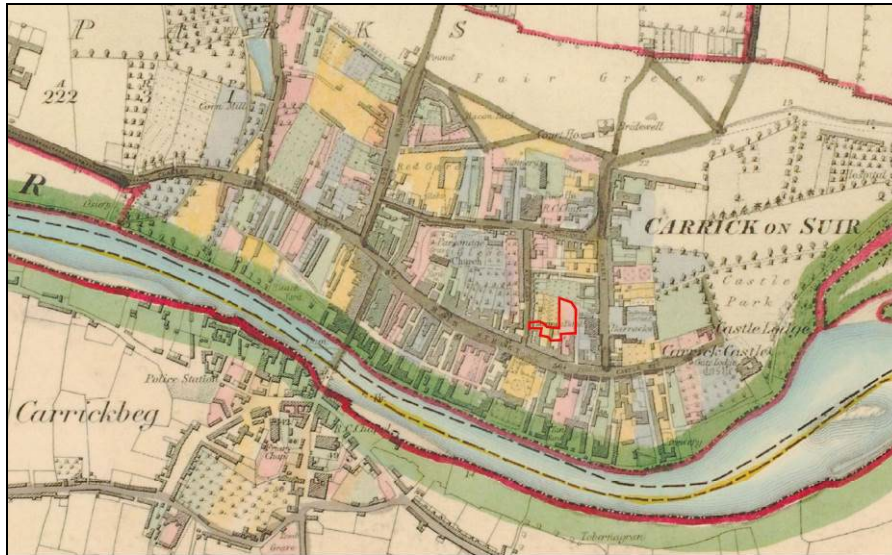


Figure 11: Composite of the Ordnance Survey First Edition 1:10,560, sheets TY085, WD003 and KK038, circa 1840.

The location of the proposed new car park is outlined in red.

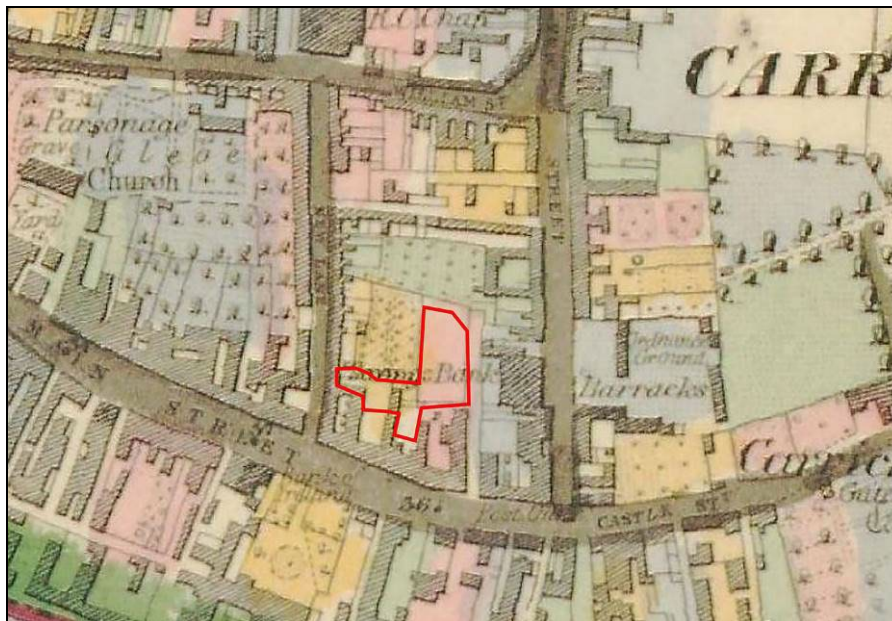


Figure 12: Detail from the Ordnance Survey First Edition 1:10,560.

Not to scale.

Stable Lane & Chapel Street (New Lane)

Stable Lane and Chapel Street (New Lane) are located within the medieval walled town area, and run northward from Main Street. Of the two, Chapel Street (New Lane) is the older.

Chapel Street runs parallel to New Street to the east, and comes through strongly on the First Edition of the Ordnance Survey mapping of 1840 (see **Figure 12**). New Lane, as it is labelled on the historic Ordnance Survey mapping, is the traditional placename for Chapel Street.

An analysis of the origins and development of Carrick-on-Suir by historian Patrick Power (Power 1992) puts forward that the north to south route of the New Line appears to fossilise

the eastern line of the earliest extents of the medieval walled town area on the island in the Suir; on which the 13th the manor of Carrick or *Carrigmacgriffin* developed. While the manor and settlement is present from the mid-13th century, it only appears to have been walled in the early 14th century (ibid. 187-189). As is often the case in the organic development of historic urban centres, it is likely that the street developed as a route immediately outside the 14th century wall, and then became fossilised in a new townscape when the walled area was enlarged to the east in the mid-15th century (ibid.) The once extra-mural route was retained as a street in the extended town. The early 14th century town wall was lengthened eastward on the north, forming the east to west running line of William Street and Town Wall Street that now fossilises the town wall's circuit in the modern townscape. The New Lane route was probably maintained as a street as it led to a secondary or postern gate in the newly extended town defences (see **Figure 13**).

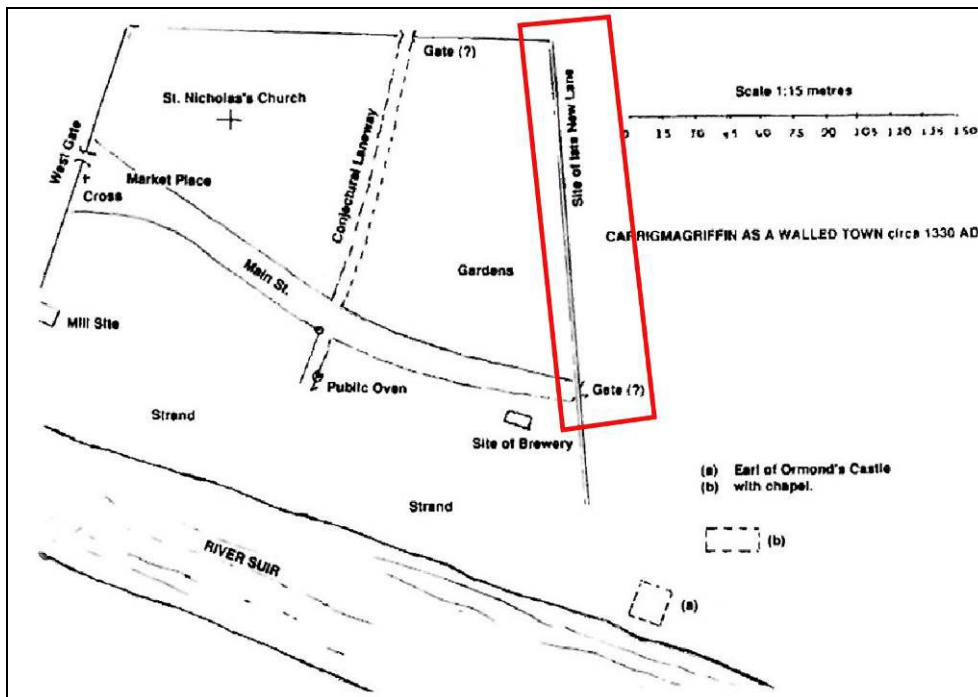


Figure 13: Patrick Power's interpretation plan of the earliest extents of the walled town of Carrick-on-Suir circa 1330.

The added red rectangle shows the New Lane. Note the possibility of the presence of a gate in a position where the present-day Chapel Street meets Main Street.

Source: Power 1992.

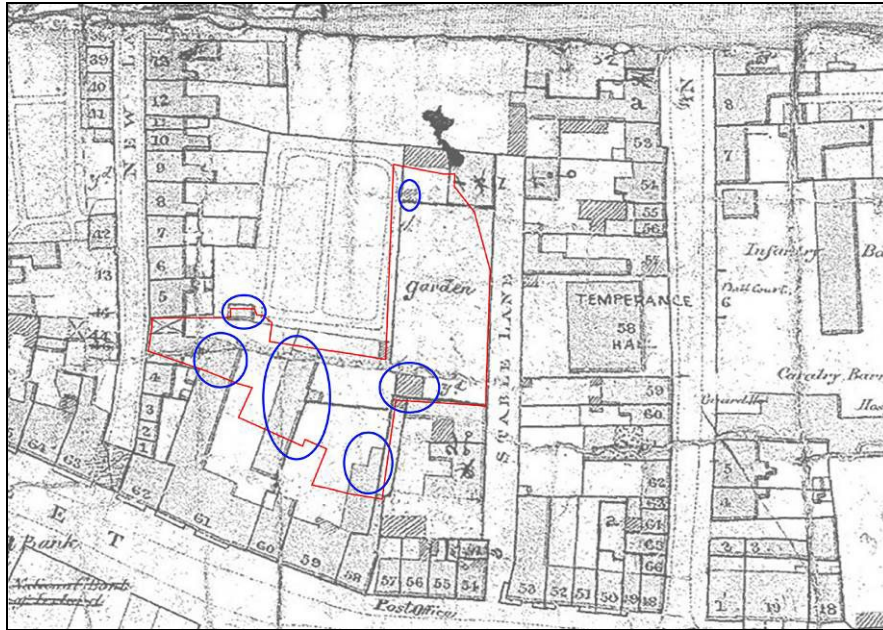


Figure 14: Extract from the mid-19th century Valuation mapping for Carrick-on-Suir, with the proposed car park location outlined in red. The buildings were historically removed are circled in blue.

Stable Lane is a shorter run of throughfare that, from the historic mapping, gave access from Main Street to the gardens and rears of many of the properties in a block formed by New Street to the east, Main Street to the south, Chapel Street (New Lane) to west, and Cross Lane to the north. It is possible that Stable Lane was an enlargement of a pre-existing smaller passageway, altered as part of the development works associated with the building of the historic early 19th century Town Hall building on the west side of New Street; to provide carriage access and stabling for horses as the placename suggests.

What is of note on the historic mapping is the presence of former buildings within the red line boundary of the proposed car park that are no longer substantially upstanding. The mid-19th century Valuation map (**Figure 14**) shows six structures of varying sizes that have been partly or totally removed. They are all present on the 1st Edition Ordnance Survey (**Figure 12**), which was surveyed in the 1830's, and it is probable that they are 18th century in origin. However, an earlier date cannot be ruled out, and archaeological mitigation will be required.

5. Site Inspection & Works Appraisal

As part of this assessment, a walkover of all areas of proposed works (**Figure 15**) was conducted, and is presented as a photographic essay, with an accompanying appraisal. The proposed development area is roughly a reversed L-shape and comprises of two parts, a Tipperary County Council Depot off Stable Lane, and an existing roughly rectangular parking facility accessed from Chapel Street (New Lane) that is located at the rear of properties fronting onto the north side of Main Street.

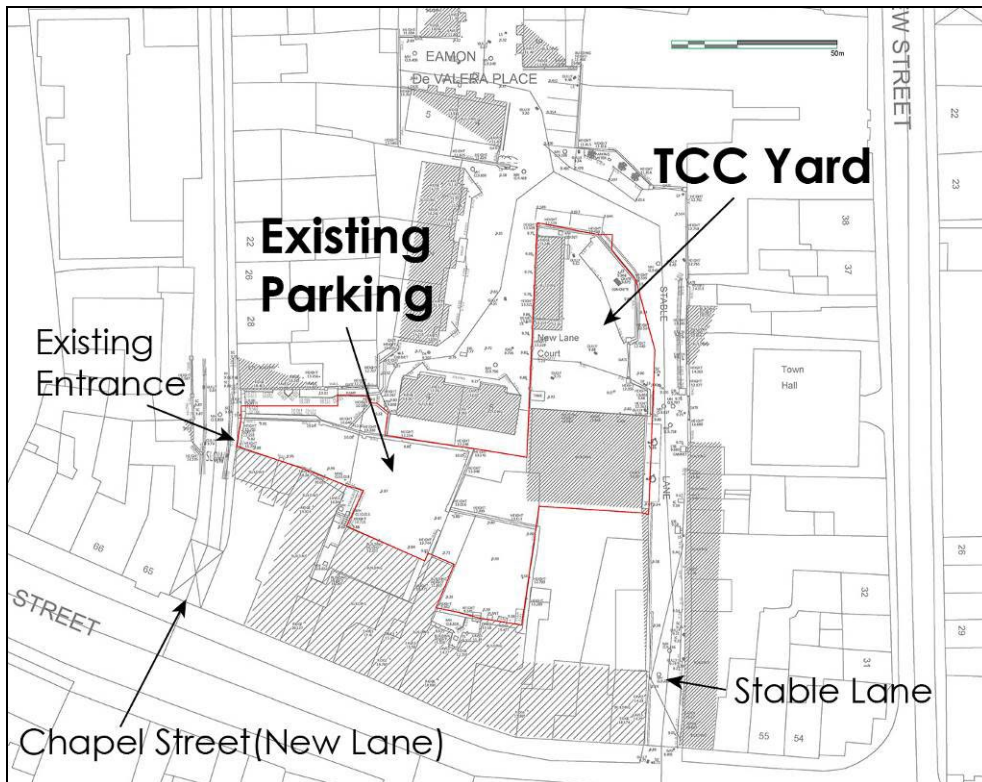


Figure 15: The existing layout of the site, within the application red line.

Note the Tipperary County Council depot off Stable Lane, and an existing parking facility that is accessed from Main Street via an entrance on Chapel Street (New Lane).

Courtesy of MWP.



Photograph 1: TCC Depot on the west side of Stable Lane, looking northwest. The green gate marks the southeast corner of the development area. The existing boundary wall is rubble stone with concrete render as far as the splayed vehicle entrance, and concrete block thereafter.



Photograph 2: Entrance to the Depot, looking southwest.



Photograph 3: Northern return to the Depot boundary, looking southeast. A speed ramp table is proposed for here. The concrete block boundary wall will be replaced.



Photograph 4: Concrete block boundary along the western side of the Depot, viewed from New Lane Court, looking east.



Photograph 5: Concrete surface Depot yard, looking north.



Photograph 6: Modern structure in northwest corner of Depot, to be demolished.



Photograph 7: Exposed stone fabric of eastern boundary wall, south of the entrance, looking east. This fabric will be removed, and requires a photographic record in advance, once the Depot shed is demolished.



Photograph 8: Depot shed, looking south.



Photograph 9: Interior of Depot shed, looking south.



Photograph 10: Cover yard, formed by stone boundary walls to southwest of the Dept, and accessed from it. This fabric will be removed, and requires a photographic record in advance, once the Depot shed is demolished.



Photograph 11: Pedestrian passageway from New Lane Court to Chapel Street (New Lane), to be partially absorbed into the new car park; and it should be included in the recommended photographic record of wall fabrics prior to demolition.



Photograph 12: Entrance to the existing parking from Chapel Street (New Lane).



Photograph 13: View into the existing parking area, looking east, towards an opening in a north to south orientated stone boundary wall that roughly divides this into two. This wall appears to be the surviving east wall of the large, free-standing structure shown on the mid-19th Century Valuation map, see **Figure 14** above.

This fabric should be included in the recommended photographic record of wall fabrics prior to demolition.



Photograph 14: View of the northern boundary wall of the parking area, easternmost section. This concrete block rendered wall will be removed. To the right of it the stone fabric, with window apertures, appears to be the surviving gable-end of another structure shown on the mid-19th Century Valuation map, see **Figure 14** above.



Photograph 15: View of the surviving gable-end of the structure in northern boundary wall of the parking area.

This fabric should be included in the recommended photographic record of wall fabrics prior to demolition.



Photograph 16: View of the northern boundary wall of the parking area. The wall to the east of the gable-end is a modern stone wall, and will be retained as part of the development works.



Photograph 17: View of the surviving east wall of the large, free-standing structure shown on the mid-19th Century Valuation map.

This fabric should be included in the recommended photographic record of wall fabrics prior to demolition.



Photograph 18: View looking north of the eastern space of the existing parking area. Note the stone fabric boundary walls, both of which should be included in the recommended photographic record of wall fabrics.

Note that this area on the mid-19th Century Valuation map is shown to contain several structures that are now gone. The subsurface survival of the foundations of these buildings, and any of the other structures/walls shown on the historic mapping for the development area is unknown, and it must be assumed pending further investigations that they remain, and require archaeological investigation in advance of removal.

6. Assessment of the Proposed Development

From the documentary and archival sources it can be concluded that the proposed new car park development between Stable Lane and Chapel Street (New Lane) is within the archaeological Zone of Notification (ZON) for the historic town of Carrick-on-Suir, reference TS085-004----. Currently, there are 33 known monuments and sites that form the known archaeological resource for Carrick-on-Suir, both upstanding and potentially subsurface. Modern town-plan analysis (Power 1992) of the streetscape and plot arrangements in the historic core has developed the hypothesis that an earlier, smaller 13th century town was delimited by the north to south route of the New Line, and it became fossilised as a new street when the area of the walled town was expanded eastward in the early 14th century. There is no known evidence for the town wall having been discovered on the line the New Line. However, its presence along the west side of Chapel Street (New Lane) cannot be discounted. The Ordnance Survey mapping evidence indicates that much of the red line footprint of the proposed car park is located in what were in the 19th, and probably earlier 18th centuries, back gardens and outbuildings such as stables associated with the surrounding buildings. The mapping also indicates the former presence of buildings within the red line extents that have historically been removed, but foundations may remain.

Site inspection of the proposed car park location did not uncover any upstanding fabric or surface indicators of potential subsurface archaeological material, including the buildings shown on the historic mapping. However, without more intrusive assessment through approaches such as archaeological test trenching, the true archaeological potential of the site cannot be fully determined.

Overall, the background research and site inspection indicates that the proposed car park development space between Stable Lane and Chapel Street (New Lane) has to be regarded as being an area of archaeological potential that requires appropriate archaeological mitigation measures, to progress the development in an archaeologically suitable manner.

If left unmitigated, ground works during the car park development have potential to directly and severely impact on the subsurface archaeological resource of Carrick-on-Suir.

In terms of visual impact, a car park development by its nature is low level, and the proposed development location for the Stable Lane and Chapel Street (New Lane) is sufficiently distant and hidden by the intervening townscape such that there will be no visual impact on Ormond Castle, a National Monument, reference 447.

7. Mitigation Recommendations¹

It is recommended that the proposed new car park between Stable Lane and Chapel Street (New Lane) Carrick-on-Suir proceed as proposed, with the following mitigation measures.

Pre-Development Archaeological Testing

It is recommended that, given the proposed works will occur in the archaeological Zone of Notification for the historic town of Carrick-on-Suir, reference TS085-004----, and the proposed car park development area between Stable Lane and Chapel Street (New Lane) is regarded as being an area of archaeological potential that requires appropriate archaeological mitigation measures, a programme of archaeological test trenching of the site should be conducted. The trenching, subject to the approval of and issuing of the appropriate licence by the National Monuments Service of the Department of Housing, Local Government and Heritage, should be conducted as a pre-development scoping exercise in advance of any construction works.

The intrusive test trenching exercise will determine the level, if any, of potential impact on the subsurface archaeological resource, and will allow for the development of an appropriate resolution strategy in consultation with the National Monuments Service, and will aid in de-risking the proposed development in archaeological terms, and possible delays to construction.

The test trenching must be conducted by an appropriately experienced archaeologist, under licence from the National Monuments Service. Sufficient time, in the order of 4-6 weeks should be allowed for securing a licence, via application to the National Monuments Service.

Should archaeological material, features or objects be encountered, Tipperary County Council should be prepared to be guided by the National Monuments Service, and the National Museum of Ireland as appropriate, on the resolution of any findings.

Record of Built Heritage

It is recommended that prior to any demolition works, a full photographic and drawn record of the standing walls for demolition should be prepared, in mitigation towards their removal.

¹ **Note on Recommendations**

All mitigation measures are recommendations only and the decision on implementation, amendments, etc. rests ultimately with the Planning Authority – Tipperary County Council, and the Development Applications Unit of the Department of Housing, Local Government and Heritage.

8. Sources & References Consulted

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- Down Survey Mapping – www.downsurvey.tcd.ie;
- Placenames database - <https://www.logainm.ie>;
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